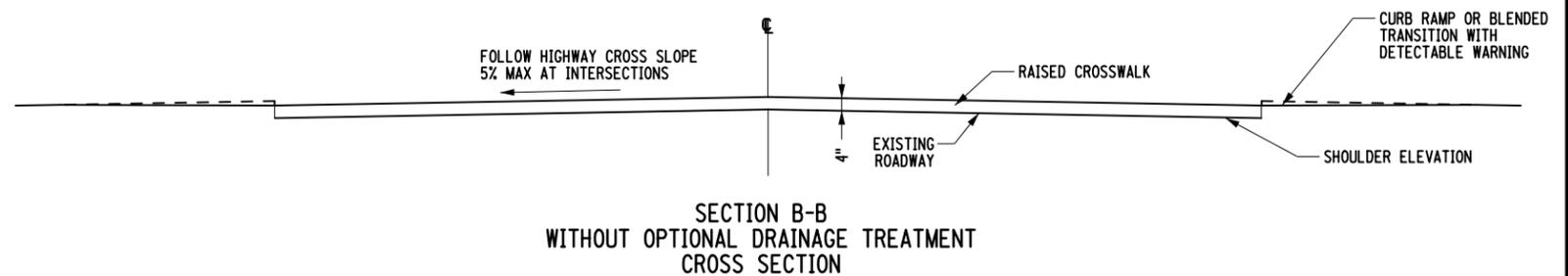
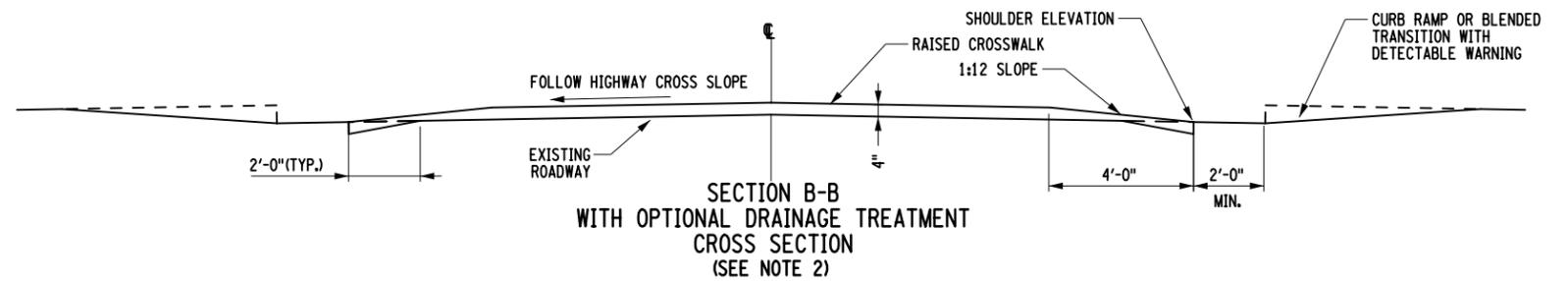
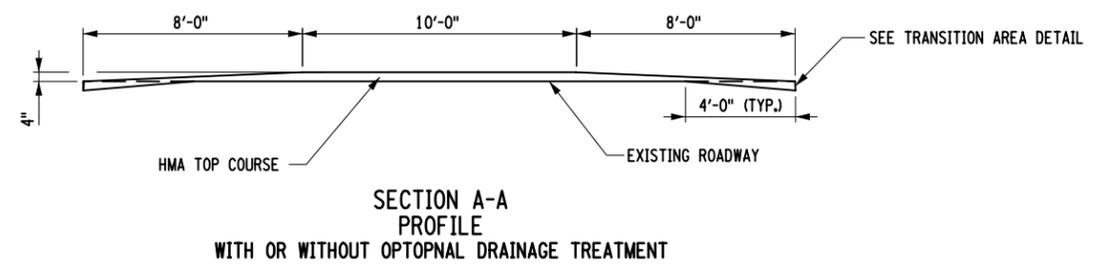
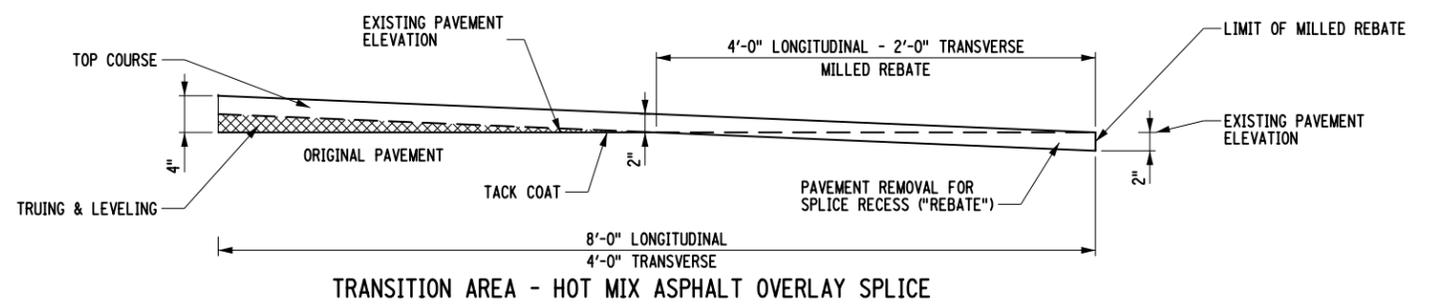
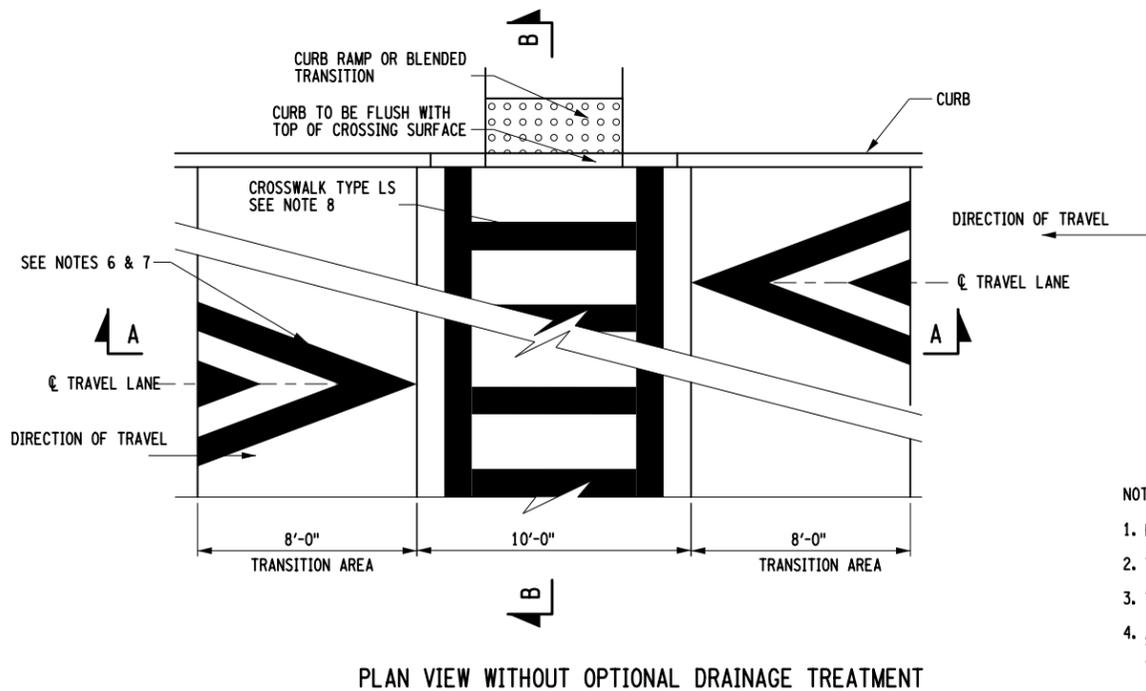
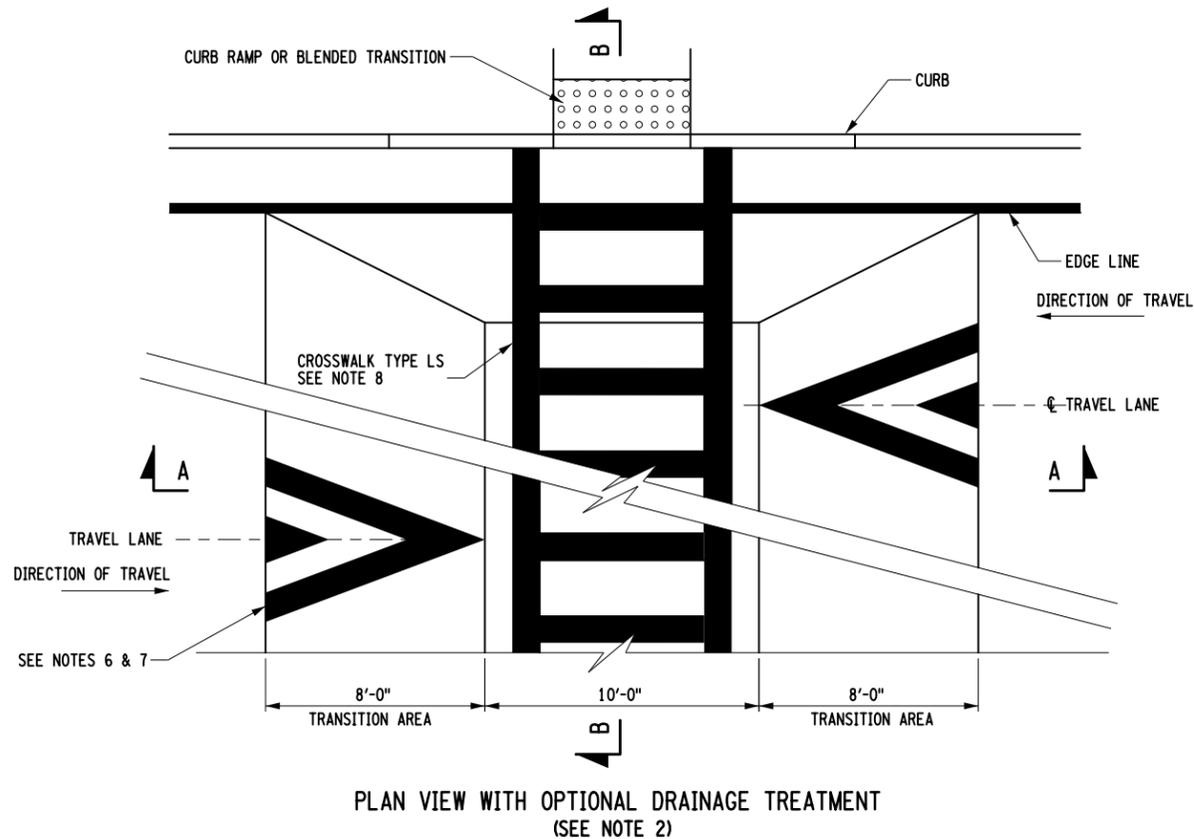


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NOTES:

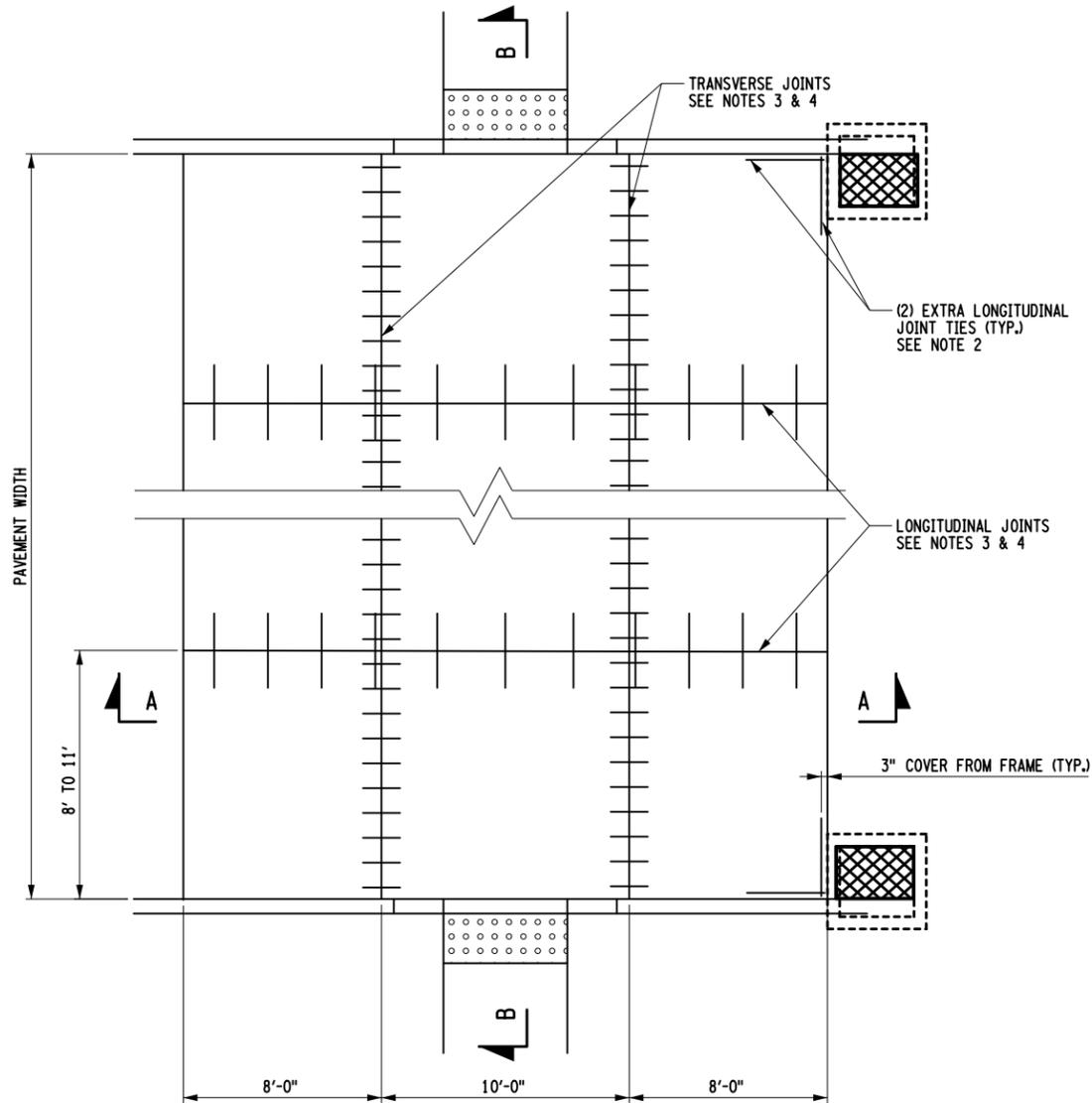
1. RAISED CROSSWALK REQUIRES STORM WATER TO BE COLLECTED AND CONVEYED TO AN APPROPRIATE LOCATION.
2. THE OPTIONAL DRAINAGE TREATMENT SHALL ONLY BE USED IN RETROFIT SITUATIONS.
3. THE TRANSITION AREA MAY BE CONCRETE OR HMA, AND WILL BE AS SHOWN ON THE PLANS.
4. ALL SURFACES OF THE MILLED TRANSITION AREA SHALL BE CLEANED. TACK-COAT SHALL BE APPLIED PRIOR TO HMA PLACEMENT. THE COST OF MILLING REBATES AND TACK COAT IN THE HMA OVERLAY SPLICE TRANSITION AREA SHALL BE PAID FOR UNDER THEIR RESPECTIVE ITEMS. THE COST OF LEAVING A NEAT EDGE SHALL BE INCLUDED IN THE MILLING ITEM.
5. SEE SHEET 2 OF 3 FOR PCC RAISED CROSSWALK DETAILS.
6. SEE SHEET 3 OF 3 FOR SIGNING AND PAVEMENT MARKING DETAILS.
7. SIGNAGE AND PAVEMENT MARKINGS SHALL BE IN ACCORDANCE WITH THE MUTCD.
8. FOR CONTRAST OR AESTHETICS, A TYPE S CROSSWALK PAVEMENT MARKING MAY BE USED WITH A COLORED OR TEXTURED CROSSWALK SURFACE.

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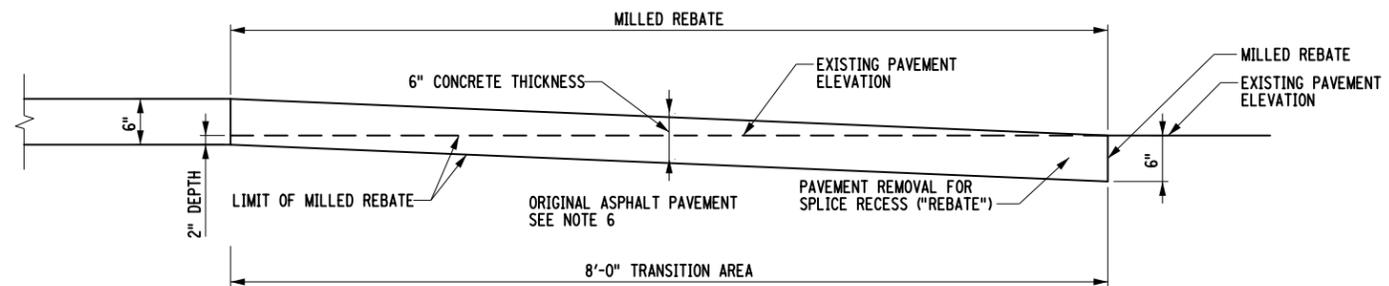
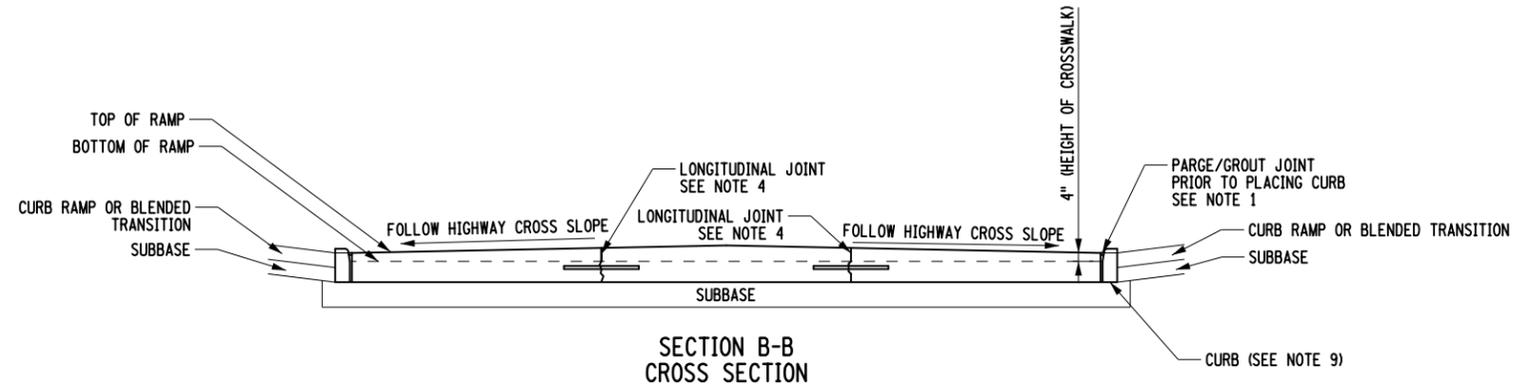
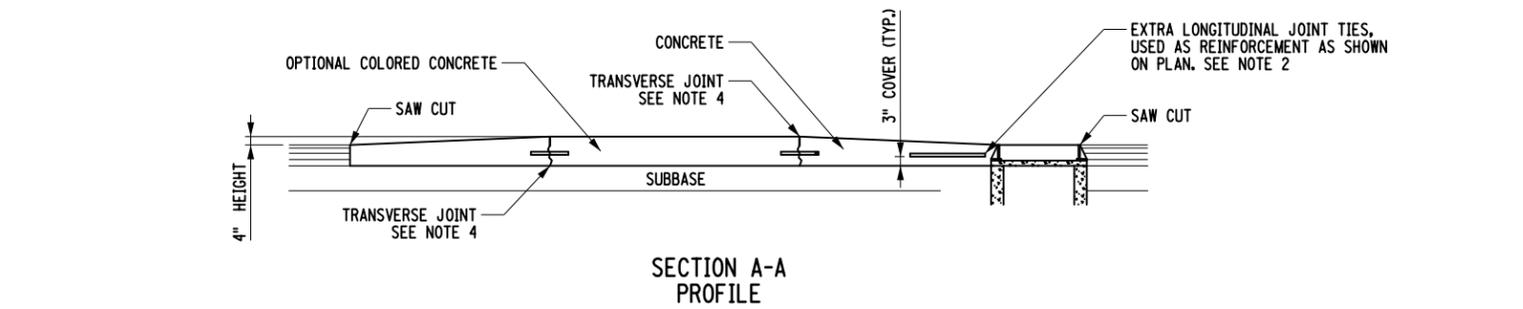
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| | STATE OF NEW YORK DEPARTMENT OF TRANSPORTATION |
| U.S. CUSTOMARY STANDARD SHEET | |
| RAISED CROSSWALK DETAILS (SHEET 1 OF 3) | |
| APPROVED 08/30/13 /S/ RICHARD W. LEE, P.E. DEPUTY CHIEF ENGINEER (DESIGN) | ISSUED UNDER EB 13-036 608-07 |

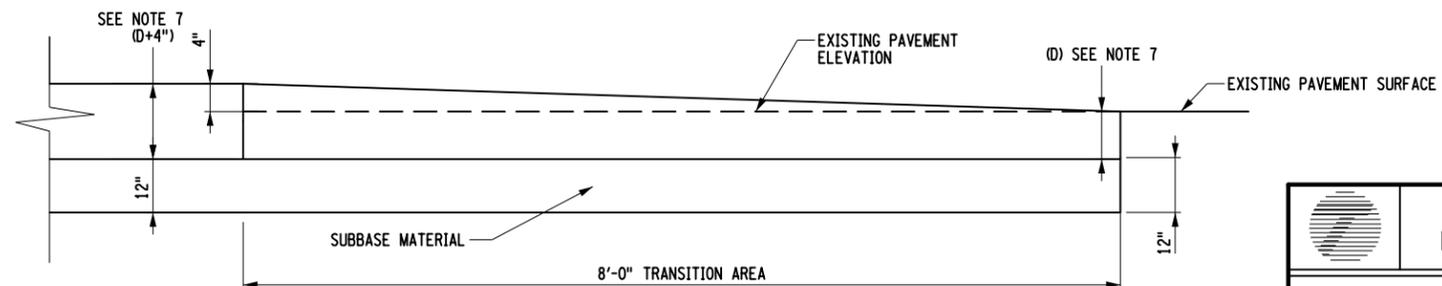
SUPERSEDED AS OF 5/1/2016 PER EB 16-032



**CAST IN PLACE
CONCRETE RAISED CROSSWALK REINFORCEMENT
PLAN VIEW
(SEE NOTE 9)**



**OPTIONAL
TRANSITION AREA FOR PCC OVERLAY SPLICE ON FULL DEPTH HMA
(SEE NOTE 6)**



**TRANSITION AREA - FULL DEPTH
(SEE NOTE 8)**

NOTES:

1. GROUT BETWEEN GRANITE/PRECAST CURBS AND CONCRETE PAVEMENT SHALL MEET STANDARD SPECIFICATION 705.21 FOR TYPE S MASONRY MORTAR. CAULKING SHALL CONFORM TO REQUIREMENTS OF CAULKING COMPOUND FOR DRAINAGE STRUCTURES.
2. EXTRA LONGITUDINAL JOINT TIES, USED AS REINFORCEMENT, ARE NEEDED ONLY WHEN DRAINAGE STRUCTURES ARE PRESENT.
3. SEE NYSDOT STANDARD SHEET 502-02 FOR PROPER DOWEL BAR/TIE BAR SPACING.
4. SEE NYSDOT STANDARD SHEETS 502-03 THROUGH 502-07 FOR LONGITUDINAL AND TRANSVERSE JOINT DETAILS.
5. SIGNAGE AND PAVEMENT MARKINGS SHALL BE IN ACCORDANCE WITH THE MUTCD. SEE SHEET 3 OF 3 FOR SIGNING AND PAVEMENT MARKING DETAILS.
6. MAY BE USED FOR HIGHWAYS WITH $\leq 20,000$ ADT, $\leq 5\%$ TRUCKS, AND A PAVEMENT SURFACE SCORE OF 7 OR GREATER.
7. DETERMINE PCC THICKNESS (D) FROM THE CONTRACT DOCUMENTS.
8. FOR PCC PAVEMENT, COMPOSITE PAVEMENTS, AND HMA PAVEMENTS NOT MEETING NOTE 6, FULL DEPTH REPLACEMENT IS REQUIRED.
9. SEE CURB & GUTTER STANDARD SHEETS FOR ANCHOR REQUIREMENTS, IF APPLICABLE.

NOT TO SCALE

EFFECTIVE DATE: 1/9/2014

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| | STATE OF NEW YORK DEPARTMENT OF TRANSPORTATION | |
| | U.S. CUSTOMARY STANDARD SHEET | |
| RAISED CROSSWALK DETAILS (SHEET 2 OF 3) | | |
| APPROVED 08/30/13 /S/ RICHARD W. LEE, P.E. DEPUTY CHIEF ENGINEER (DESIGN) | NOT TO SCALE EFFECTIVE DATE: 1/9/2014 | ISSUED UNDER EB 13-036 608-07 |

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